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Kilsby Neighbourhood Plan

July 2021

Design Code

Quality information

Document name	Ref	Prepared for	Prepared by	Date	Reviewed by
Kilsby Neighbourhood Plan Design Code	DR-11213	Kilsby Neighbourhood Plan Steering Group (NPSG)	Elliot Joddrell, AECOM	02/08/2021	Lee Wood, AECOM

Revision history

Revision	Date	Details	Authorised	Name	Position
00	11/05/2021	1st Draft issued to NPSG	LW	Lee Wood	Regional Director
01	07/06/2021	2nd Draft reflecting comments received from Ian Massey	LW	Lee Wood	Regional Director
02	22/06/2021	3rd Draft including Design Codes Section	LW	Lee Wood	Regional Director
03	06/07/2021	4th Draft reflect comments received in engagement session held on 30th June 2021	LW	Lee Wood	Regional Director
04	30/07/2021	Final Document signed off by Locality	LW	Lee Wood	Regional Director





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01 Introduction

Background

The Parish Council of Kilsby in the unitary district of West Northamptonshire have established a Neighbourhood Plan Steering Group (NPSG) in order to shape and influence development within their area. The NPSG have updated their made Neighbourhood Plan to incorporate Design Codes.

Locality is the national membership network for community organisations that brings local people together to produce Neighbourhood Plans. Through Locality's support programme, Kilsby NPSG have appointed AECOM to prepare this Design Code document which will form part of the evidence base for their Neighbourhood Plan Review.

Objective

The purpose of this document is to provide an appreciation of Kilsby Parish's existing character and to create a set of design codes which will apply to any future housing development. This will help to ensure that as any new development comes forward, it responds to its context and supports and enhances the quality of the village's existing character.

Methodology

The process that was undertaken to produce this Design Code document is as follows:

- On the 24th February 2021, an inception call was held with AECOM representatives and the NPSG to understand the aims of the group and confirm the brief.
- On the 15th April 2021, AECOM representatives carried out a site walkover in Kilsby Parish in order to appreciate the local character and photograph the area.
- On the 11th May 2021, AECOM shared a draft Design Code document with the NPSG.
- On the 22nd June 2021, AECOM shared the final draft Design Code document with the NPSG
- On the 30th June 2021, an engagement meeting was held with the NPSG to review the draft document and allow local opinion to be captured and represented in the final document.
- After capturing the feedback from the engagement meeting, AECOM issued the final Design Code on 22nd July 2021.

Document Structure

This Design Code document comprises the following five sections:

01 Introduction

Outlining the background, purpose, process, study area and design code document structure.

02 Place Assessment

Provides an appreciation of physical influences which will be used to help inform the design codes.

03 Local Character

A more focussed understanding of Kilsby Parish's built and natural landscape character is provided by undertaking a photographic survey to analyse key characteristics.

04 Design Codes

The design codes to be applied to future housing developments in the Neighbourhood Plan area are established.

05 Next Steps

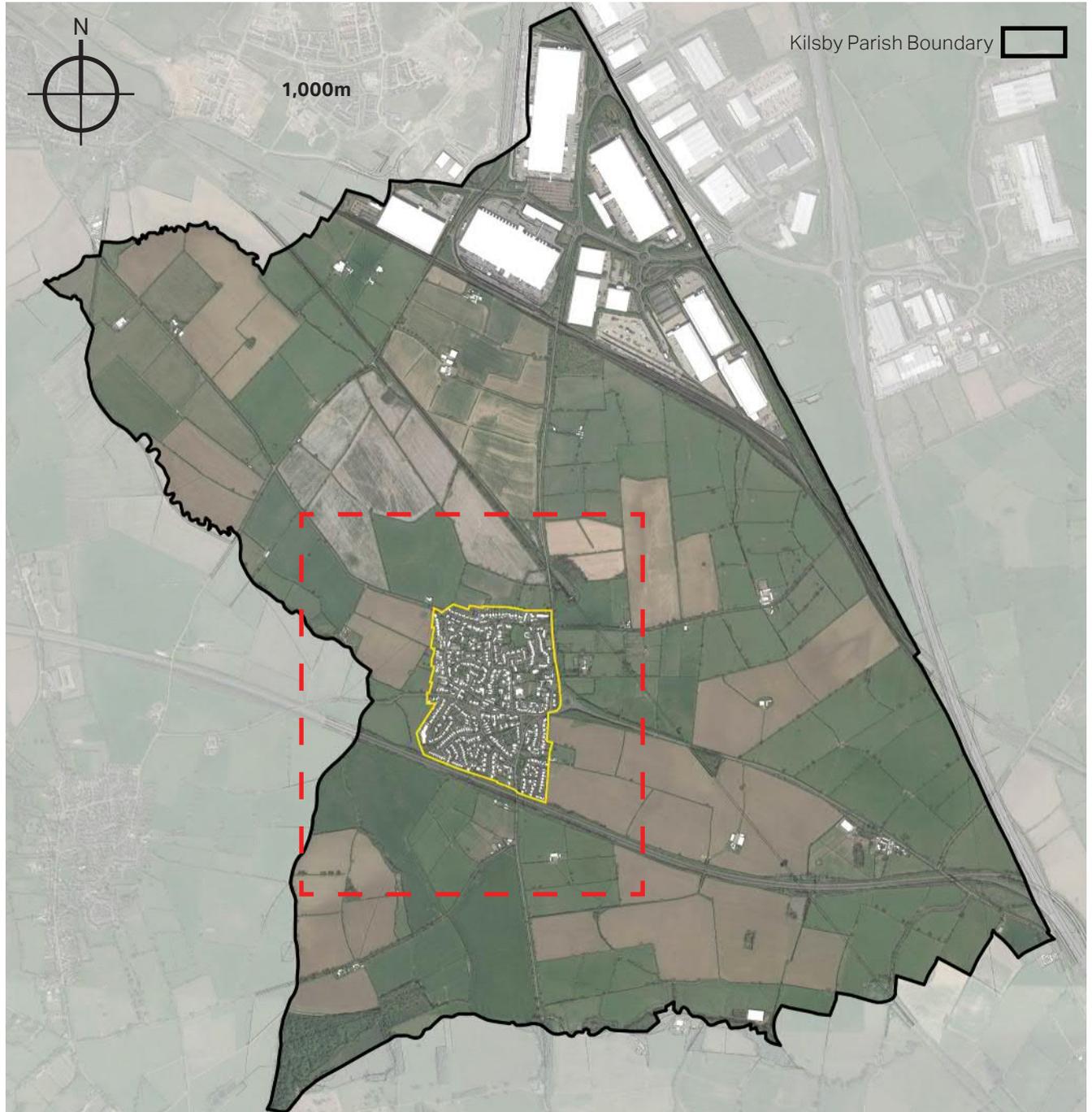
Provides guidance on the next steps for the NPSG and potential future developers.

Study Area

The Neighbourhood Plan area / Parish Boundary comprises the village of Kilsby and the surrounding rural context. In order to influence the design of future housing around the main settlement area, this document will predominantly be focussed on the extent of the village and its more immediate surroundings.



Kilsby's settlement boundary



Kilsby Neighbourhood Plan area and settlement boundary



Watling Street

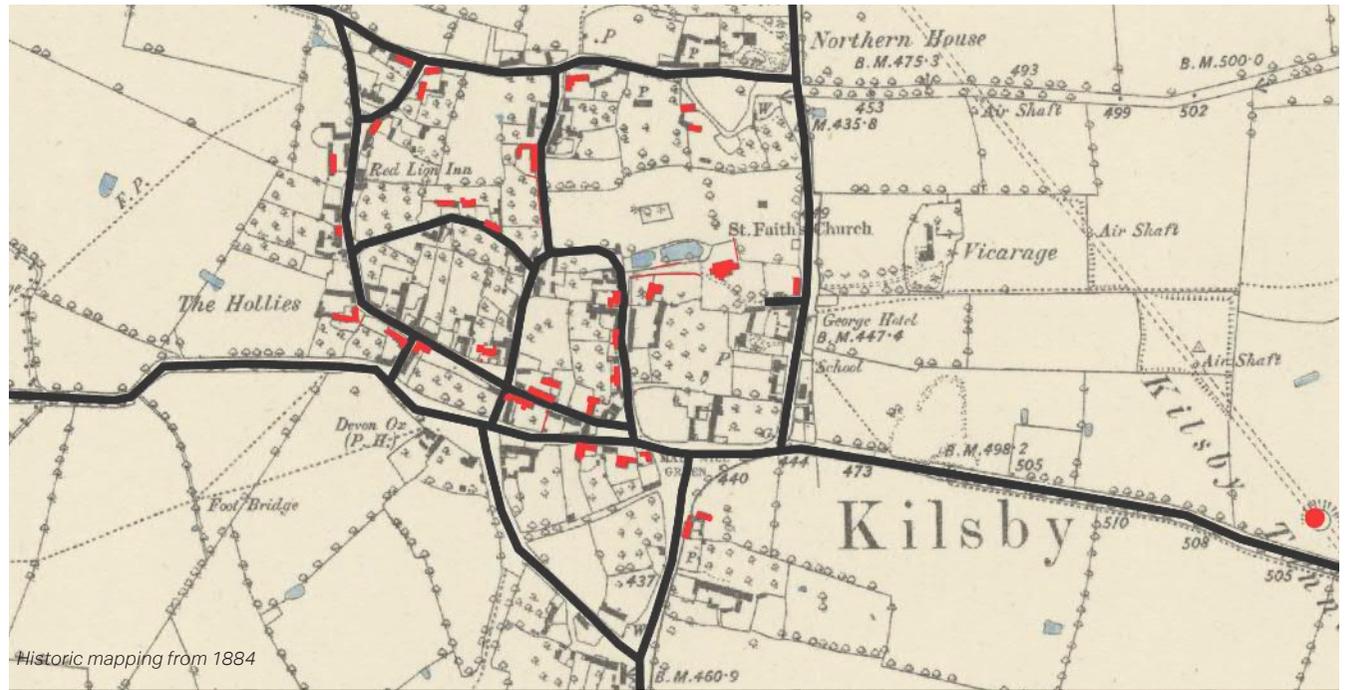
02 Place Assessment

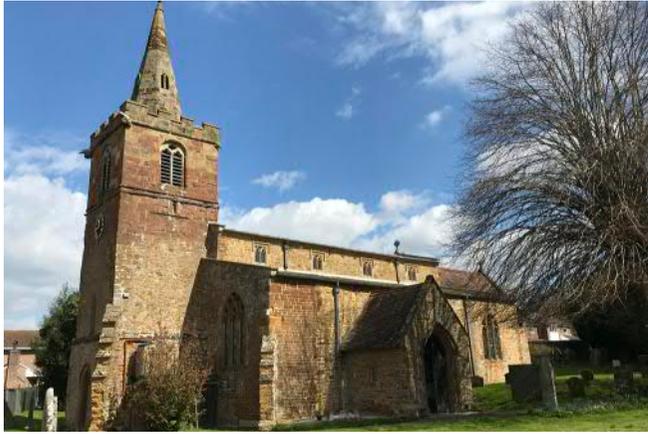
Historic Evolution & Heritage

The historic settlement pattern that Kilsby developed from comprises a series of looped routes which have formed the blocks from which strong building frontages face the street. Later phases of the village's development have been laid out in more densely developed cul-de-sac streets which, in some cases, have diminished the historic character of the village. These cul-de-sac routes have reduced the levels of connectivity that the village once had.

The village's conservation area comprises Chapel Street, Main Road, Manor Road, Essen Lane and St Faith's Church. The majority of the village's listed buildings are located within the conservation area.

Kilsby has a wealth of historic buildings. The photographs on the opposite page are a small selection of the listed buildings within the area which contribute to the historic character of the village. The location of these buildings is illustrated on the adjacent plans. In addition to the listed buildings on the opposite page, key land marks within the parish are the grade II* listed castellated ventilation shafts of the Kilsby railway tunnel which was built in 1833 (see below).





Grade II* Listed Church of St Faith



Grade II Listed Northgate House



Grade II Mansfield Cottage



Grade II Listed Moat House Farmhouse



Grade II Listed Japonica



Grade II Listed The Limes



Grade II Listed Lyn Cottage & Rippon Cottage



Grade II Listed The Haven



Grade II Listed The Forge

Route Hierarchy

Kilsby is located to the south of Rugby and is a 5 minute drive from Junction 18 of the M1. The M45 runs along the southern edge of the village to join the M1 at Junction 17.

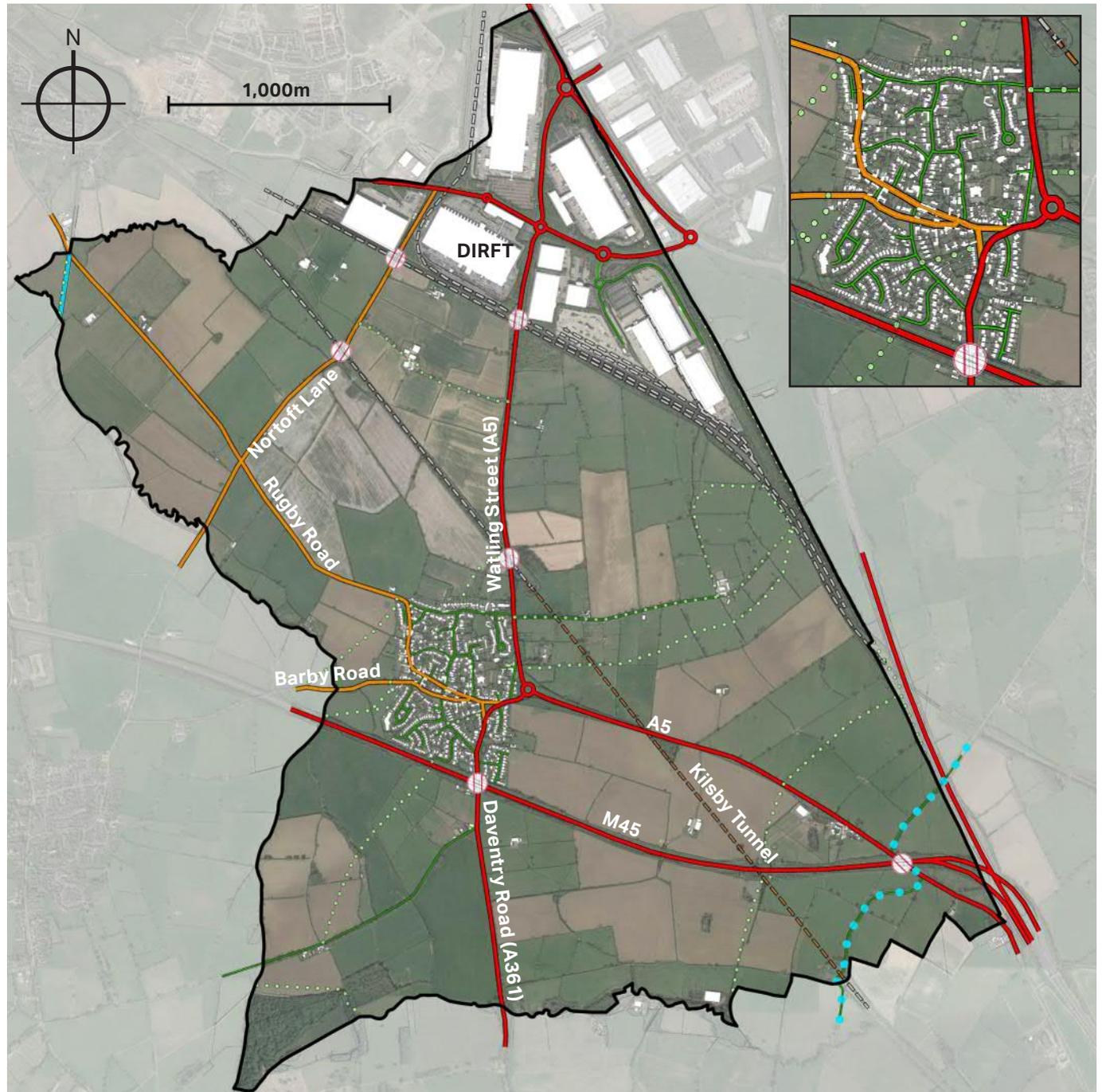
The streets in the parish comprise of a mix of historic connecting routes following historic field boundaries and modern cul-de-sac access streets and estates.

The primary route through the village is Watling Street (A5) which runs from Daventry International Rail Freight Terminal (DIRFT) in the north to the north west edge of the village where it connects to the Daventry Road (A361) trunk road which leads to Daventry in the south. This route is heavily used by freight traffic linking to the M40 at Banbury as well as the logistics parks in Daventry.

The historic secondary routes which run through the village are Rugby Road and Barby Road which provide access from north-west and south-east of the village.

The surrounding area is well connected with public right of way routes providing traffic free footpaths to explore the surrounding landscape. In addition, National Cycle Network Route 50 runs through the south-east corner of the neighbourhood plan area. This route when completed will connect Maidenhead with Winslow.

- Primary route —
- Secondary route —
- Access route —
- Public right of way ⋯
- Railway
- Railway tunnel
- Bridge ⊘
- National Cycle Network Route 50 ● ● ●
- Canal —



Route Hierarchy



M45



Watling Street (A5)



Barby Road



Main Road



PROW leading to Barby



Rugby Road



PROW leading north from Rugby Road



PROW leading south from Rugby Road



Butts Lane PROW providing access from Daventry Road to Ashby Road

Village & Open Space Structure

The village is comprised of a compact settlement area which has evolved from a series of historic routes. The majority of the local amenities are found in the main settlement area including: The Church of St Faith, the United Reformed Church, outdoor sports and recreation spaces (Kilsby Village Park, Play Area and Tennis Courts) and allotments. There are two public houses in the village - The Red Lion and The George which was closed at the time of the site visit. The village also has a primary school. Many of the streets in the village have pavements on only one side and with the other side of the street lined with grass verges. This along with the many areas of amenity green spaces reinforce the rural character of the village.



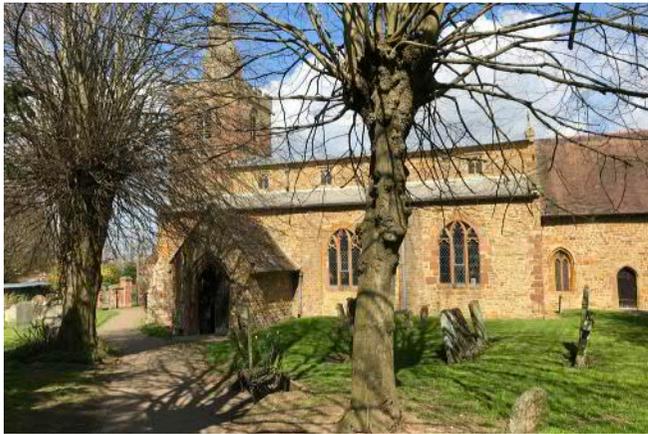
Village Shop located in The Red Lion car park



Tennis Courts



Village Centre Structure and Open Space



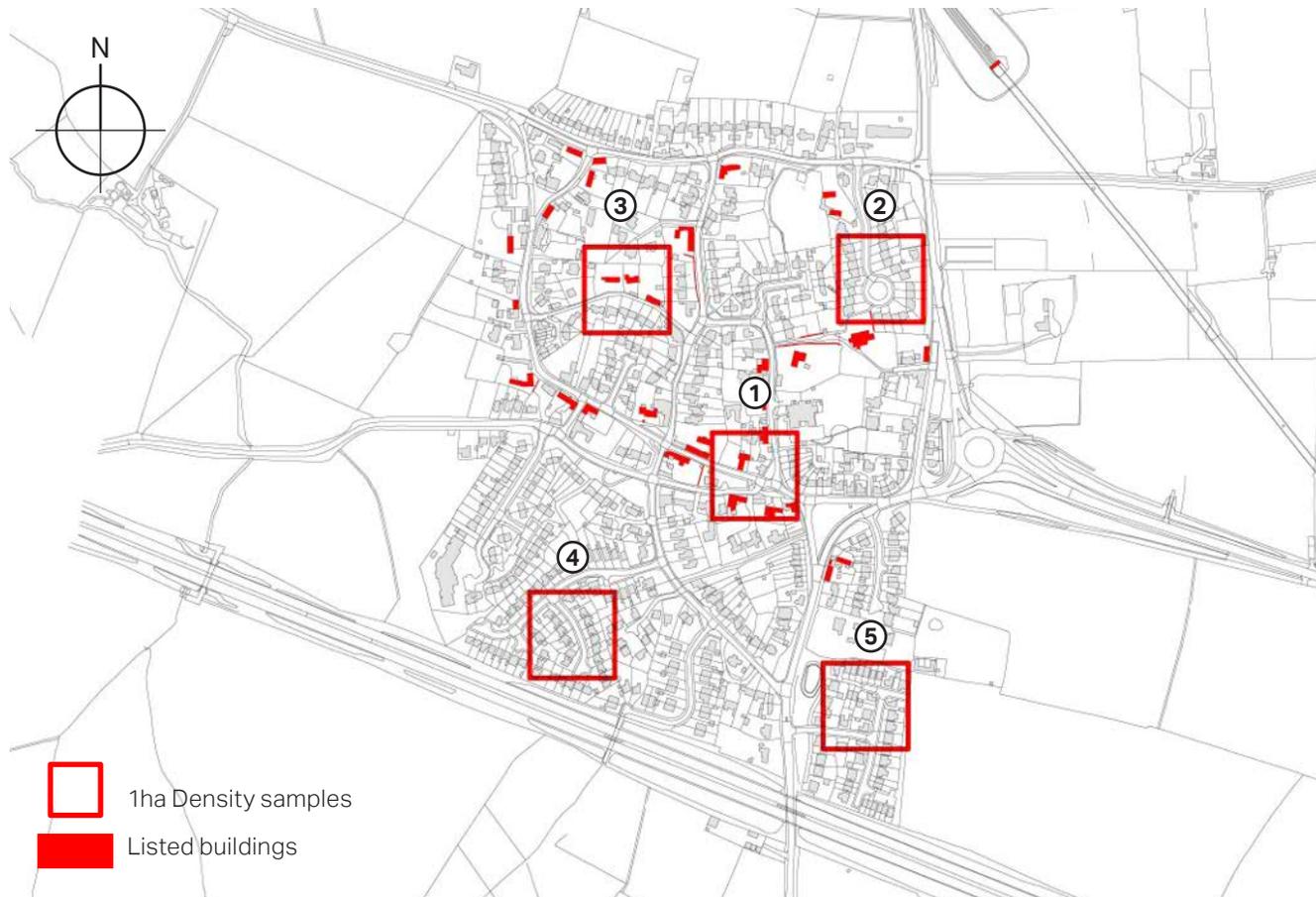
St Faith's Church



The Red Lion



Kilsby Village Hall



Kilsby 1 hectare housing density samples

On the adjacent plans the following 1 hectare housing density samples have been tested to understand the typical density of development in Kilsby.

1. Main Road / Manor Road / Chapel Street - 13 dwellings per hectare (dph)
2. Hall Close - 23dph
3. Essen Lane - 10dph
4. Cowley Way - 20dph
5. Normandy Fields Way - 24dph

This shows that in the historic parts of the village (density samples 1 and 3) densities are typically lower at 10 and 13dph. The growth of the village has resulted in some edge of settlement densities that are much higher densities than the historic development pattern (density samples 2, 4 & 5). This can have an adverse impact on the surrounding landscape and future developments should have lower densities to provide a softer transition to the rural area.

Sense of Place & Wayfinding

The undulating landscape character surrounding Kilsby has resulted in several view points from where the surrounding landscape can be appreciated (see views 1 and 5).

A key area of importance in the village is the junction of Chapel Street, Main Road, and Manor Road from where most of the village can be accessed.

The central routes through the village have a strong historic feel with most of the historic buildings in the area lining these routes.

There are a number of historic listed buildings which contribute to the character of the village. The Grade II* Listed St Faith's Church is a prominent landmark which can be seen from many locations across the area (see views 5 and 8). The Grade II* Kilsby Tunnel ventilation shafts are architecturally unique landmarks which can be seen when arriving at the village by car on the A5.

The influence of the surrounding landscape contributes to the rural character of the village.

- Open space and recreation
- Public right of way
- Listed buildings
- Key views
- Landmarks
- Key area of importance



Sense of Place and Wayfinding



View 1 - Looking south-west from Barby Road



St Faith's Church local landmark



View 2 - Looking north from Rugby Road



View 3 - Looking north from a public right of way off Rugby Road



View 4 - Looking south-west from a public right of way off Rugby Road



View 5 - Looking north from the public right of way off Watling Street (A5)



Kilsby Tunnel ventilation shaft local landmark

Landscape

The surrounding landscape is predominantly open farmland with scattered farms and hedgerow and tree bound fields. To the south of the village there are a small number of wooded areas including Briccle Wood and Ashby Home Wood which surround Barby Reservoir.

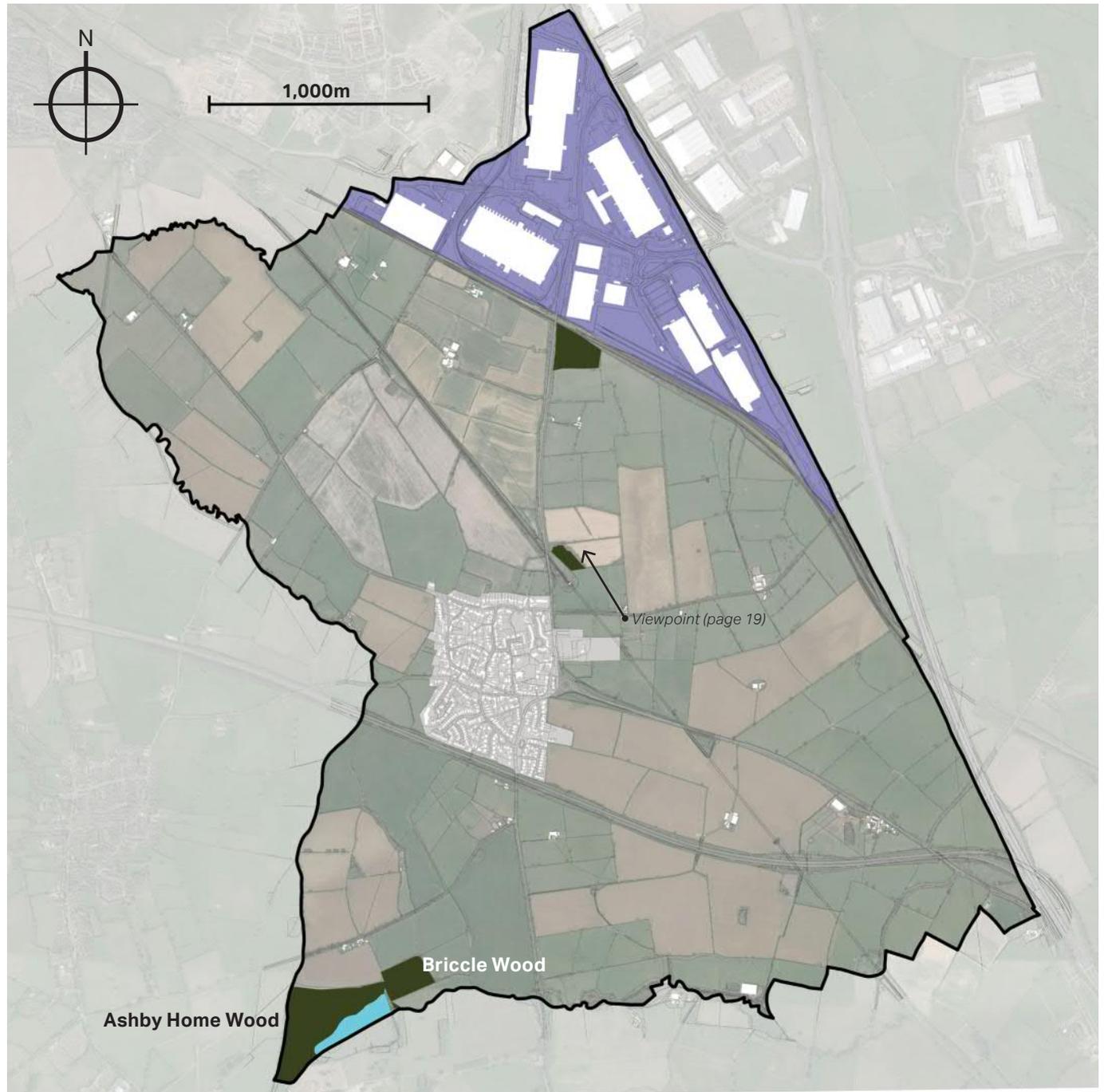
The settlement area is a compact area of development which sits within the rural setting. To the north of the parish the Daventry International Rail Freight Terminal (DIRFT) comprises large floor plate industrial buildings and rail infrastructure.

Within the Daventry Landscape Character Assessment, 2017 Kilsby lies within Landscape Character Type 13. Undulating Hills and Valleys and Landscape Character Area 13b Daventry. This notes that, 'Villages are generally on rising ground such as Barby and Kilsby which overlook the neighbouring Broad Unwooded Vale (LCA19b).

Within the Northamptonshire Historic Landscape Character Assessment 2015 Kilsby sits within Historic Character Area 5e. Crick Undulating Clayland, where significant amounts of ridge and furrow survive throughout the area. This is a prominent feature within the landscape surrounding the village.

The parish sits within Natural England's National Character Area 95: Northamptonshire Uplands. The area is described as 'an area of gently rolling, limestone hills and valleys capped by ironstone-bearing sandstone and clay Lias, with many long, low ridgelines'.

- Woodland
- Barby Storage Reservoir
- DIRFT
- Settlement area



Landscape



DIRFT



Looking north of the village



Essen Lane

03 Local Character

Photographic Analysis & Observations

A site walkover of the village was undertaken on the 15th April 2021 to understand the topography, structure and character of the village.

Following the visit, a photographic analysis has been prepared to identify and illustrate key design features which help underpin Kilsby's intrinsic character. This understanding has been used to inform and shape the application of the proposed design codes.

The images which have been chosen for the purpose of this analysis represent a sample from across the village as illustrated in the adjacent key plan. Each of the images portrays a key characteristic / spacial responses which help to shape the sense of place.



Kilsby Photo Analysis Key Plan



1. Manor Road - Mixed material palette, 3 storey building height



2. Rugby Road - One sided pavements, grass verges, red brick materials



3. Ofield Lane - Grass verge boundary treatments



4. Rugby Road - Planted boundary treatments, ironstone materials



5. Manor Road - Parking, grass verge, steep thatched roof



6. North Street - Changes in orientation, landscaping slate roofs, mixed materials



7. Rugby Road - Edge of settlement screening



8. Middle Street - Brick wall boundary, ironstone building materials



9. Main Road - Parking to side, thatched roof



10. Rugby Road - Pathways providing access to surrounding landscape



11. Chapel Street - 2.5 storey building height



12. Middle Street - Steep roof pitches and use of roof space to create habitable rooms / rear parking access



13. Ashby Road - Hipped roof



14. Rugby Road - Hipped roof forms



15. Barby Road - Grass verges and large areas of amenity green space

Key Findings

- The settlement edge is softened and screened with tree planting and back gardens which supports a soft transition from the settlement to the surrounding landscape.
- Pathways provide accessibility to the surrounding landscape.
- Typical building heights across the parish are 2 storeys which should be responded to in future development, but it is appropriate to create additional storeys within the roof space of a dwelling. There are some 3 and 2.5 storey dwellings which add variety and interest to the village's roofscape. Roofs of older dwellings tend to have much steeper pitches which adds to the variety.
- There are a small number of hipped roof buildings but roofs generally comprise gable forms.
- The varied and informal orientation of buildings has created visual interest on some of the streets in the village.
- There are many examples of parking being provided out of sight, to the side and behind dwellings, which reduces cars cluttering the streets .
- A mixed material palette of ironstone, sandstone, red brick, limestone, render and painted brick with slate roofing is most appropriate for development in the parish. Thatched roofing is frequently used on older cottages and where feasible should continue to be used in the future.
- There are some examples of streets with pavements on only one side of the street. These often have grass verges on the other side of the road which strengthens the village's rural character and connection with nature.
- There are numerous turfed amenity green spaces which strengthen the rural and green feel of the village.
- Front and side boundaries often comprise a simple planted grass verge or planted landscaped building apron. Other boundaries include hedgerows and stone or brick walls.



Main Road

04 Design Codes

When to Use the Codes

The table below identifies when each of the codes should be used. A prefix has been created for each code to allow simple referencing and application of the design codes.

The Codes

Based on the understanding gained in the previous sections, this section will identify design codes for future housing developments to adhere to. The following design codes have been created to apply to the whole parish:

Code 1 - Sustainability and Climate Change

Code 2 - Landscape, Views and the Settlement Edge

Code 3 - Building Design

Code 4 - Parking, Gardens and Boundary Treatments

Code 5 - Kilsby Conservation Area

Code	Prefix	When to use the code
Sustainability and Climate Change	1A	Code to be applied to all future housing developments in Kilsby Parish to reduce water wastage and flood risk and tackle climate change.
Landscape, Views and the Settlement Edge	2A	Code to be applied where a building heights and the space between dwellings could impact upon views to the surrounding landscape.
	2B	Code to be applied when a proposed housing development has the potential to impact on the transition space between the developed settlement area and the surrounding landscape.
	2C	Code to be applied where a housing development could impact upon views towards the village from the surrounding landscape.
	2D	Code to be applied when a proposed housing development has the potential to restrict views to local landmarks.
Building Design	3A	Code to be applied when determining the height and scale of future housing developments.
	3B	Code to be applied when determining the material and detailing palette to be used in a housing development.
	3C	Code to be applied when the layout and orientation of a development is being designed.
Parking, Gardens and Boundary Treatments	4A	Code to be applied when designing how parking will be provided within future housing developments.
	4B	
	4C	
	4D	
	4E	Code to be applied when designing the back gardens of future housing developments.
	4F	Code to be applied when selecting the boundary treatments to be used within future housing developments.
Kilsby Conservation Area	5A	Code to be applied when considering small changes in and within the context of Kilsby Conservation Area.

Table 1: When to use the codes

Code 1 - Sustainability and Climate Change

1A - Sustainability & Energy

Any new development in Kilsby Parish should mitigate its impact from the loss of countryside, wildlife and the natural environment and demonstrate that it is responding to climate change with the highest standards of insulation and energy conservation.

- Cavity wall and under floor insulation should avoid where possible heat loss through thermal bridging. Double or triple glazing, window and door draft sealing should reach Passivhaus standards wherever possible.
- All proposals must demonstrate sustainable surface drainage systems that will not unduly increase pressure on existing wastewater and natural drainage systems.
- Gardens and parking areas should have the majority of their area landscaped, with permeable surfacing used on hard landscaped areas to enable rainwater absorption and reduce the rate of run off caused by development.
- New development should provide suitable and safe storage for bicycles of sufficient size. At least one secure space should be provided per dwelling in a garage of a suitable size or separate covered area within plot. Covered and secure cycle storage units are preferred but where enclosures are open suitable racks or hoops should be provided.
- Solar, heat recovery, air source and ground source energy is encouraged in new development and should be designed to have a minimal visual impact on a development. Where technologies have a visual impact on sensitive areas (such as solar shingles and photovoltaic slates within or close to the setting of a heritage asset) they should be designed in from the start of the scheme. Designs should aim to conceal wiring and infrastructure and use carefully chosen slates or tiles on roofs to complement the solar panel materials. Where groups of housing are proposed they should demonstrate energy efficient heating through a combined heat and power system.
- The orientation of buildings and roof pitches should incorporate passive solar design principles and allow for efficient solar energy collection. One of the main glazed elevations of future dwellings should therefore keep within 30° of south, when in keeping with the topography and clustering of existing buildings. Where it would be inappropriate for the main glazed elevation to be facing south or within 30 degrees of the this for the reason outlined above, every attempt should be made to design the roof so that is aligned to allow for the fitting of solar panels. This applies to all future dwellings whether solar panels are proposed or not to allow for retrospective implementation.
- New housing should demonstrate how rainwater and greywater will be stored and reused to reduce demand on mains supplies.
- The installation of water butts within new residential developments is encouraged to collect rainwater from roofs and reduce the overall rainwater run off impact of any development.
- Where existing buildings are being converted or extended every effort should be made to introduce energy saving measures and new technologies to make the building more efficient and sustainable.
- Whenever possible, developments should aim to re-use existing materials or procure reclaimed and recycled materials from local suppliers. Building materials made from construction and demolition waste are preferred to primary aggregates. Many types of construction waste can be used for these purposes including soil, asphalt, concrete, bricks and tiles. In conversion schemes roof tiles and slates should be carefully stored and re-used. In addition, priority should be given to materials that can be deconstructed and re-used at the end of the building's usable life.
- Ensure all proposed planting is sympathetic with existing native species profile whilst seeking to enhance diversity to provide resilience to climate changes, pests and diseases;
- Gardens and boundary treatments should be designed to allow the movement of wildlife and provide habitat for local species.
- The adoption of swift bricks, bat and owl boxes are encouraged to help provide nesting and roosting spaces or bats and birds.
- The use of green roofs and/or living walls is encouraged. These can assist with insulation and summer cooling requirements. They can also be readily integrated with solar systems and have even been shown to increase the efficiency of PV cells on hot summer days.
- Open spaces should be located within walking distance of residential areas and linked through a series of green networks or corridors. Such linkages support a Green Infrastructure approach to development, allowing wildlife to move along corridors to access foraging opportunities and habitats and people to access a range of different recreational facilities.

Where a proposal falls short of these sustainable measures it must be explained why and what compensatory measures are being offered.



Precedent image - Optimising permeability in front gardens (2)



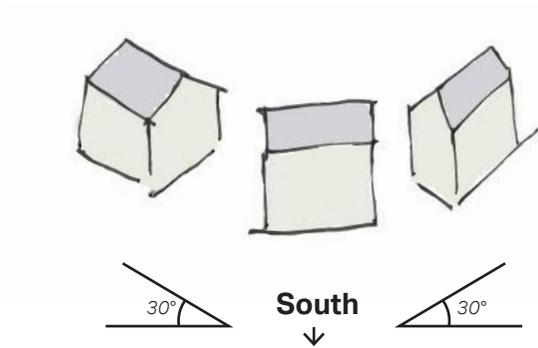
Precedent image - illustrating integration of sustainable urban drainage solution:



Precedent image - Solar tiles used to minimise visual impact (4)



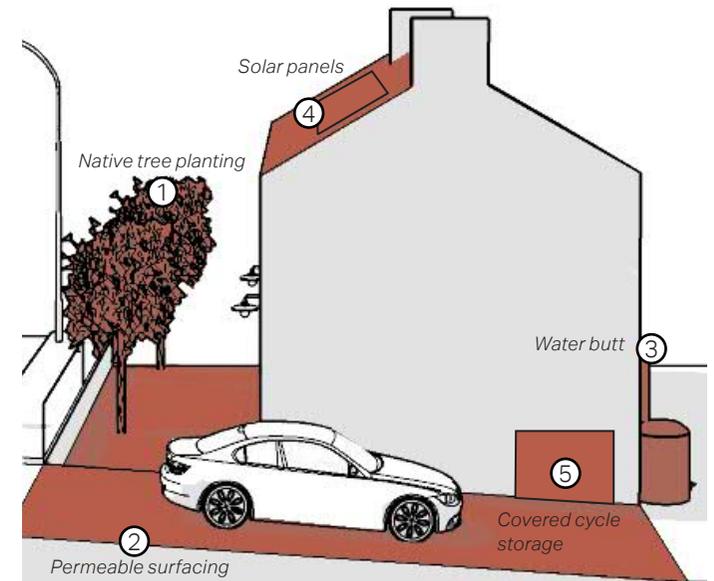
Precedent image - safe, convenient and covered home cycle storage (5)



Building/roof orientation for solar gain



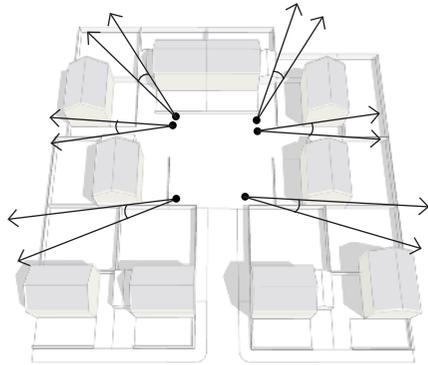
Rainwater harvesting - collection and re-use (3)



Code 2 - Landscape, Views and the Settlement Edge

2A - Views from the Village

New residential developments should be designed to have a minimal impact on existing views to the surrounding landscape. Where a development has the potential to obscure existing views to the surrounding landscape lower building heights should be proposed and/or development should ensure that there is sufficient spacing between dwellings to provide long glimpsed views, using articulation, massing and height to frame views.



Spacing between dwellings should retain views to the surrounding landscape

2B - Edge of Settlement

The most sensitive area to development is the settlement edge, at the interface between developed and rural environments. Any future development within Kilsby should be focussed within the existing settlement boundary. Any development at the settlement edge should be of a lesser density than the rest of the settlement area in order to achieve a soft and graduated transition in to the rural landscape.

Where appropriate, access to the surrounding landscape should be designed in to future development, connecting to the network of existing public rights of way surrounding the village.

2C - Amenity Landscape

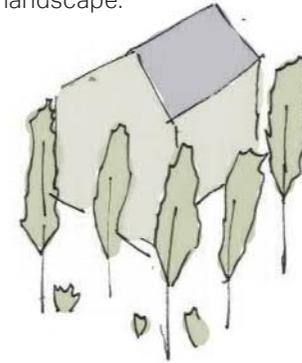
Kilsby retains a strong connection with the natural environment with an abundance of grass verges and amenity green spaces. The high volume of green areas across the village reinforces the connection with the surrounding landscape. This pattern should be adhered to in future development in order to avoid the degradation of the villages rural character.



Amenity green spaces and grass verges are typical in Kilsby

2D - Views to the Village

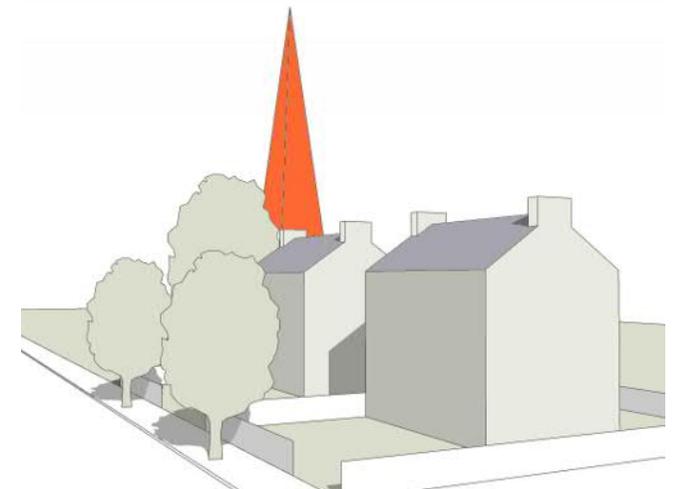
New residential developments should be well integrated with existing and/or proposed landscaping such as hedgerows and tree planting to provide natural screening and reduce the visual impact of development on the surrounding landscape.



Views to the village from the surrounding landscape should be protected by providing natural screening through the use of hedgerows and trees.

2E - Views to Local Landmarks

New developments should respect the existing shape and rhythm of skylines and new buildings should not obscure views to local landmarks.



Views of important landmarks should be protected



Development at the settlement edge should have lower densities to achieve a soft transition in to the surrounding landscape

Code 3 - Building Design

3A - Height and Scale

Future development should adhere to a maximum height of two storeys. It is acceptable for a dwelling to provide an additional storey within the roof space and use sky lights and/or gable end windows,

The scale of future development should be informed by adjacent dwellings. Neighbouring properties should be used to create a building envelope for future developments to adhere to.

3B - Materials and Detailing

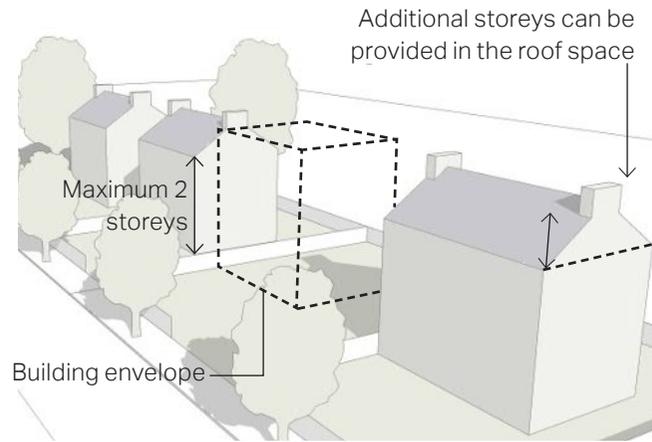
Informed by the local vernacular, the adjacent diagram illustrates acceptable materials and detailing for future housing developments in Kilsby. Future developments should carefully apply this code to avoid creating a pastiche of the existing local vernacular. Detailing can be interpreted using contemporary methods to avoid this.

Changes in materials must support the overall three-dimensional form of the buildings. Architectural detailing must be balanced to avoid both excessive range of materials/ details and at the other extreme blandness.

The choice and composition of materials must provide variety but in a controlled fashion that expresses an identity to its locality



Orientation - Buildings have an organic layout which reduces the impression of a formally planned settlement.



Height and Scale

3C - Orientation

The varied orientation of buildings in Kilsby contributes to the local character. This should be replicated in any future developments to avoid rigid layouts where buildings are clustered very formally.

Gable roof



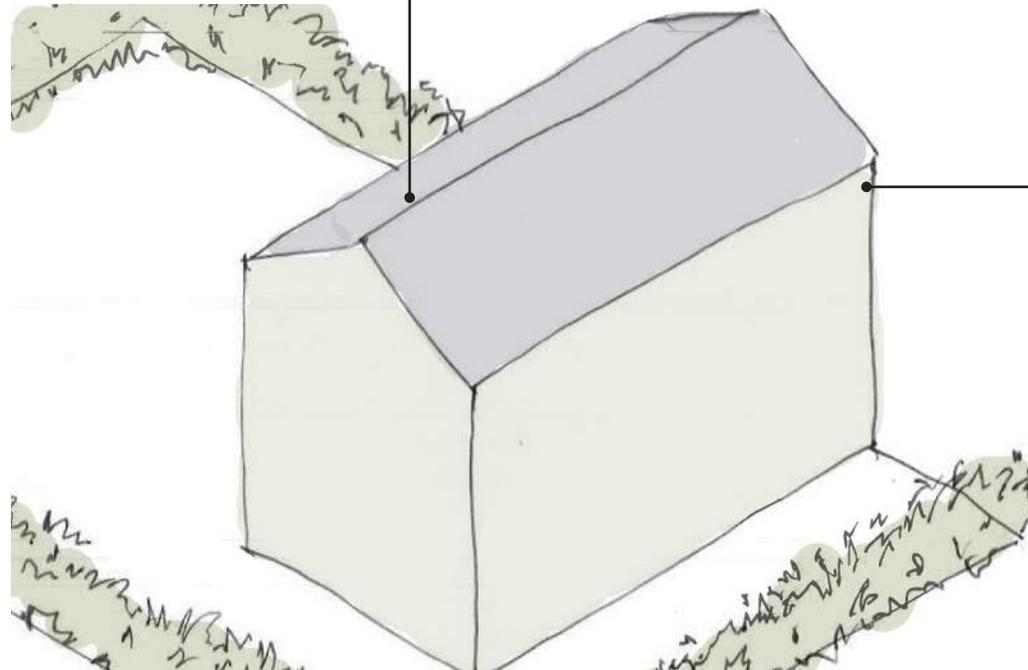
Sky lights



Slate roof tiles



Thatched roofing



Materials and detailing palette

Sand Stone



Red Brick



Iron stone



Render



Sash or casement windows



Code 4 - Parking, Gardens and Boundary Treatments

The adjacent diagram illustrates the different ways that parking can be appropriately provided within future housing developments. 1 or 2 bedroom dwellings should provide at least 1 on-plot parking space. Dwellings with 3 or more bedrooms should provide 2 on-plot parking spaces.

4A - On-Street Parking

In order to reduce the visual impact of parked cars on the street, on-street parking as the only means of parking should be avoided in future development.

Where on-street parking is delivered, it should be provided in small groupings to reduce its impact and presence on the street-scape. Landscape features and SuDs should be provided intermittently to help integrate it into the street-scene. Demarcation of on-street parking should be sensitive to the local setting, with white lines being avoided where possible in favour of more subtle and appropriate methods, such as changes in hard landscaping materials.

4B - Front of Dwelling Driveway Parking

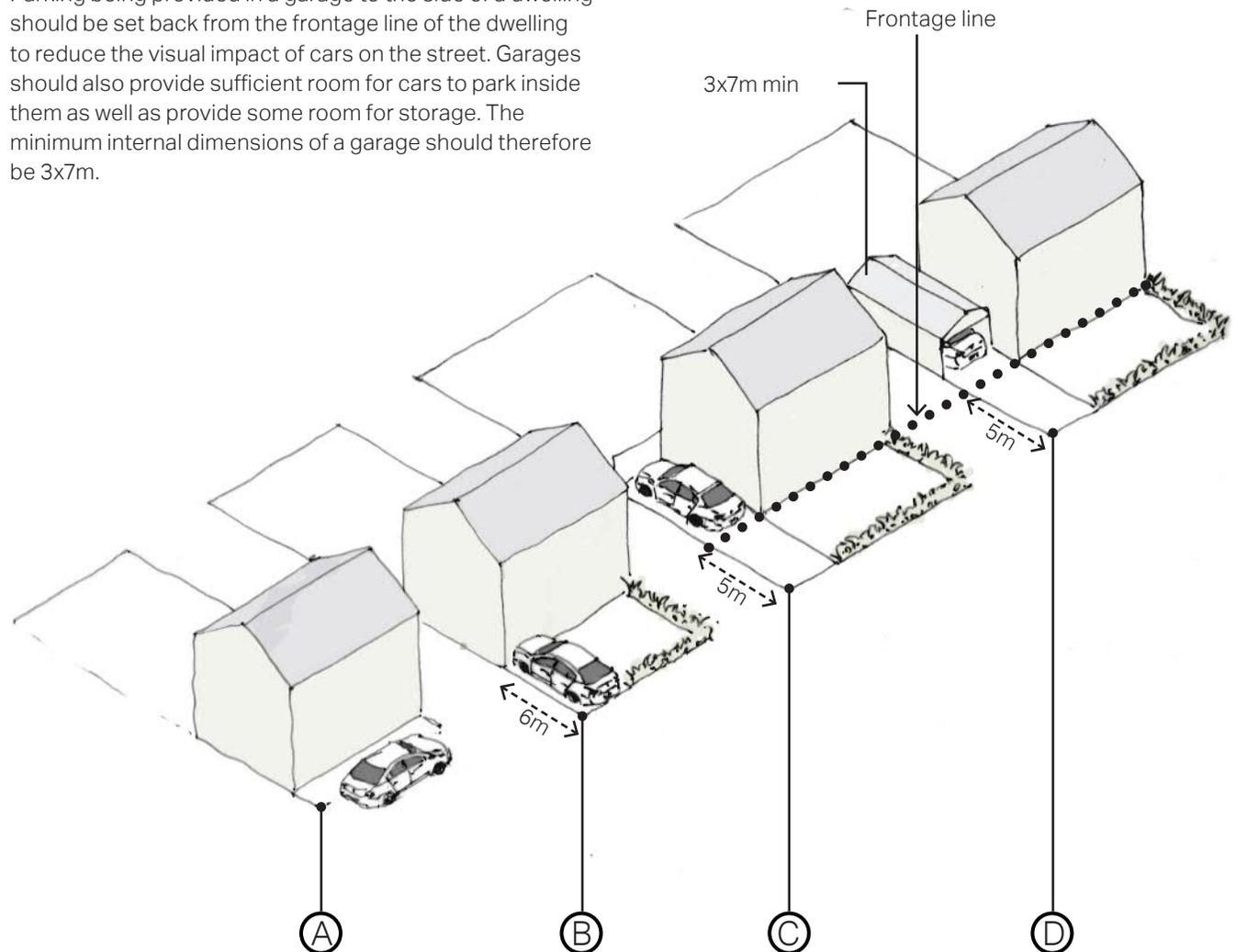
Parking provided on driveways directly in front of dwellings should be restricted due to the visual impact that cars have on the street. Therefore, a maximum of 2 dwellings in a row will be permitted to provide parking in this way. Front gardens should be a minimum depth of 6m to allow movement around parked vehicles and also be well screened with hedgerows when providing parking space to the front of a dwelling.

4C - Side of Dwelling Driveway Parking

Parking being provided on a driveway to the side of a dwelling should be of sufficient length (5m minimum) so that a car can park behind the frontage line of the dwelling. This will reduce the visual impact that cars will have on the street scene. When parking is provided to the side of a dwelling a minimum front garden depth of 3m should be provided,

4D - Garage Parking

Parking being provided in a garage to the side of a dwelling should be set back from the frontage line of the dwelling to reduce the visual impact of cars on the street. Garages should also provide sufficient room for cars to park inside them as well as provide some room for storage. The minimum internal dimensions of a garage should therefore be 3x7m.



Parking

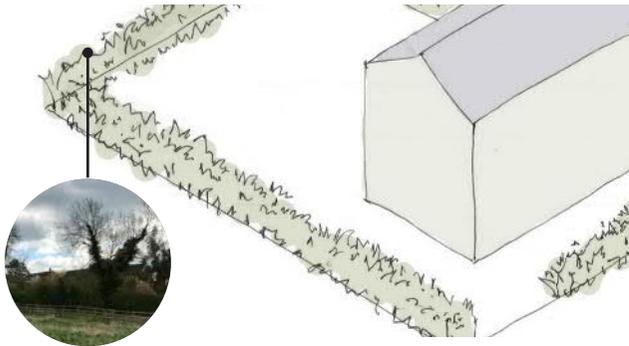
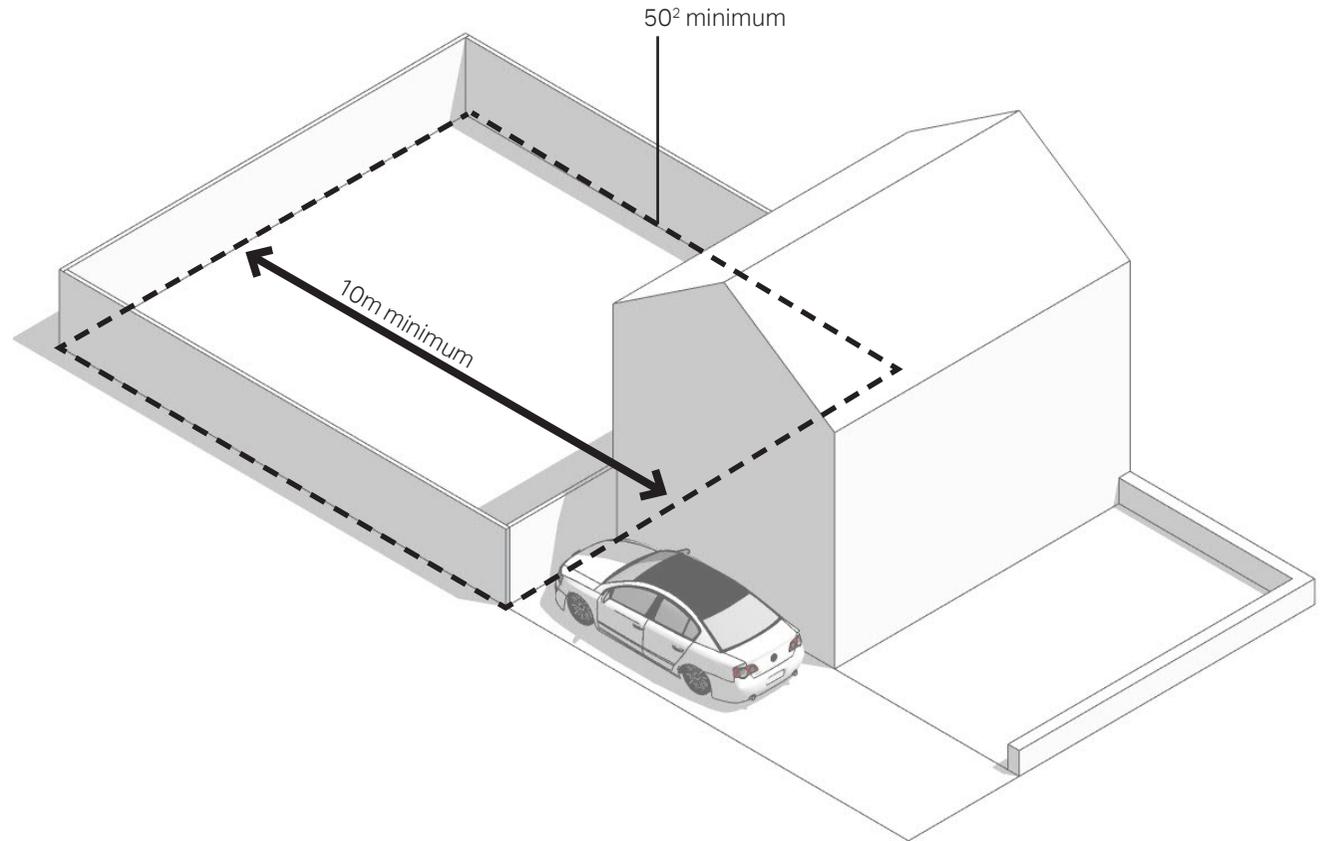
4E - Back Gardens

Back gardens should be a minimum depth of 10m and provide a minimum area of 50m² of usable amenity space. North facing back gardens should exceed 10m in length to ensure sunlight is maximised.

4F - Boundary Treatments

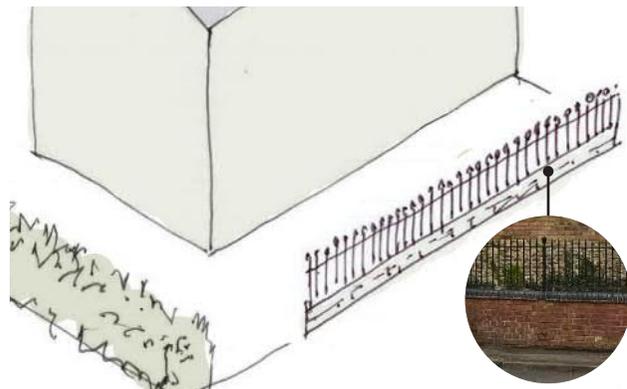
When rear boundaries abut the settlement edge, surrounding landscape or open green spaces soft planted boundaries of hedgerows and trees must be used to soften the transition into the natural environment and protect views.

Front boundaries should respond to the boundaries used within adjacent dwellings to provide continuation of street character. Appropriate boundary choices are illustrated below.

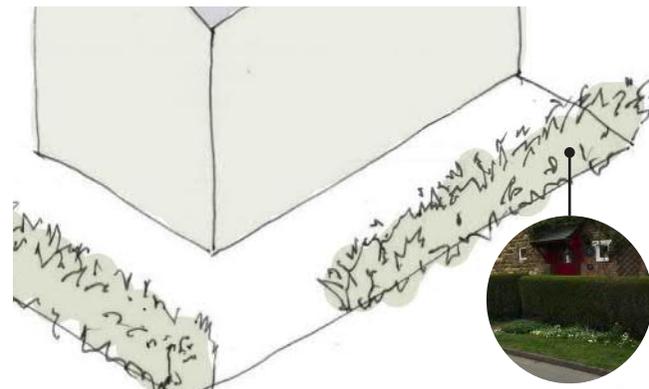


Planted rear boundaries abutting surrounding landscape

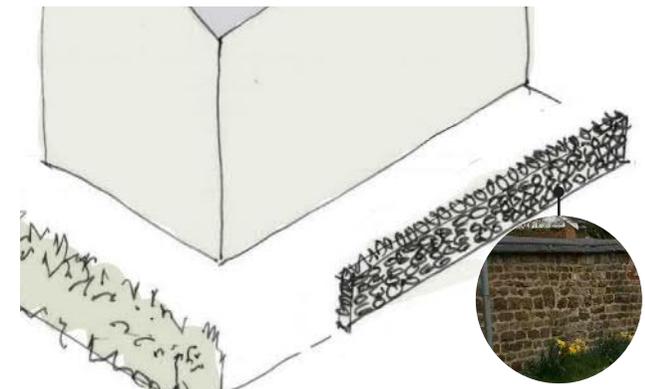
Back garden dimensions



Brick wall with or without railings



Hedgerow



Stone wall

Code 5 - Kilsby Conservation Area

Kilsby Conservation Area and Management Plan 2018 (CAAMP) highlights some of the key issues to managing small changes within the conservation area.

Positive buildings and locally listed buildings identified in the CAAMP are particularly sensitive to change. The retention of these building's architectural and special features is encouraged to uphold the character of the conservation area.

The CAAMP identifies architectural details that can be found across the conservation area which should be preserved and enhanced these are illustrated on the opposite page and include:

- Steeply pitched roofs (both thatched and formerly thatched)
- Stone and cob walling
- Flemish bond brick principle elevations
- Garden wall bond brick boundary walls
- Stone quoined building corner detailing
- Projecting stone window sills
- Timber lintels
- Stone mullions
- Timber mouldings around front doors
- Timber panelled doors
- Timber sash and casement windows
- Carriageway entrances
- Thatched or shallow timber, lead or slate canopies

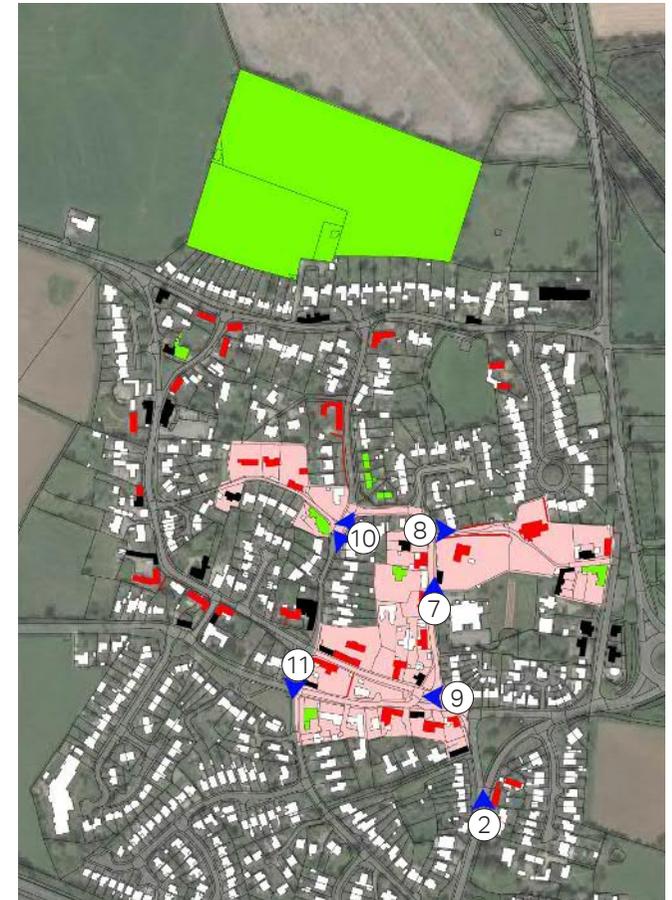
Replacement of any of these features should be carefully considered to avoid harming the historic character of the conservation area. Inappropriately detailed UPVC doors, windows, porches and canopies can harm the character and appearance of the conservation area and will not be appropriate. Porches are generally not a feature of the conservation area and will also not be appropriate.



Aerial photograph showing the historic Ridge and Furrow landscape to the north of the village.

'Important views' 2, 7, 8, 9, 10 and 11 identified within the CAAMP are all views within, into and out of the conservation area and should be preserved and enhanced.

The CAAMP identifies an area of land within the setting of the conservation area to be added to the Local List. This land is shown on the adjacent plan and above aerial photograph and is of historic archaeological and landscape importance. The Northamptonshire Historic Landscape Character Assessment 2015 identifies the land as an area of Ridge and Furrow. This historically important area of the surrounding landscape should be preserved.



- | | |
|---|--|
| Kilsby Conservation Area | |
| Listed Buildings | |
| Locally Listed buildings and land | |
| Positive Buildings | |
| Important views within, into and out of the Conservation Area | |



Principle brick elevation in Flemish Bond



Brick boundary wall using a Garden Wall Bond



Stone and Cob walling

Thatched canopy



Quoined corner detailing and stone window mullions and surrounds



Timber panelled door and moulding, timber casement windows with stone sills



Timber sliding sash windows



Carriageway entrance



Timber lintel

Timber moulding door surround



Steep pitched formerly thatched roofs

Next Steps

This document has set out an evidence base for the Kilsby Neighbourhood Plan and it is recommended that the codes are embedded within the forthcoming plan as policy.

As well as providing certainty to the local community, the design codes in this document should give more certainty to future developers, as they will be able to design a scheme that is reflective of community aspirations, potentially speeding up the planning application process.

As well as using this document, future developers should also make sure that they have observed the guidance in the Ministry of Housing, Communities & Local Government's **National Design Guide**.

Developers should also note that housing developments of any size should strive to achieve carbon neutrality in line with the Government's **Future Homes Standard**.

Further standards on residential developments should also be obtained from **Building for a Healthy Life**, a government-endorsed industry standard for well-designed homes and neighbourhoods.



View of Kilsby from Ashby Road



